

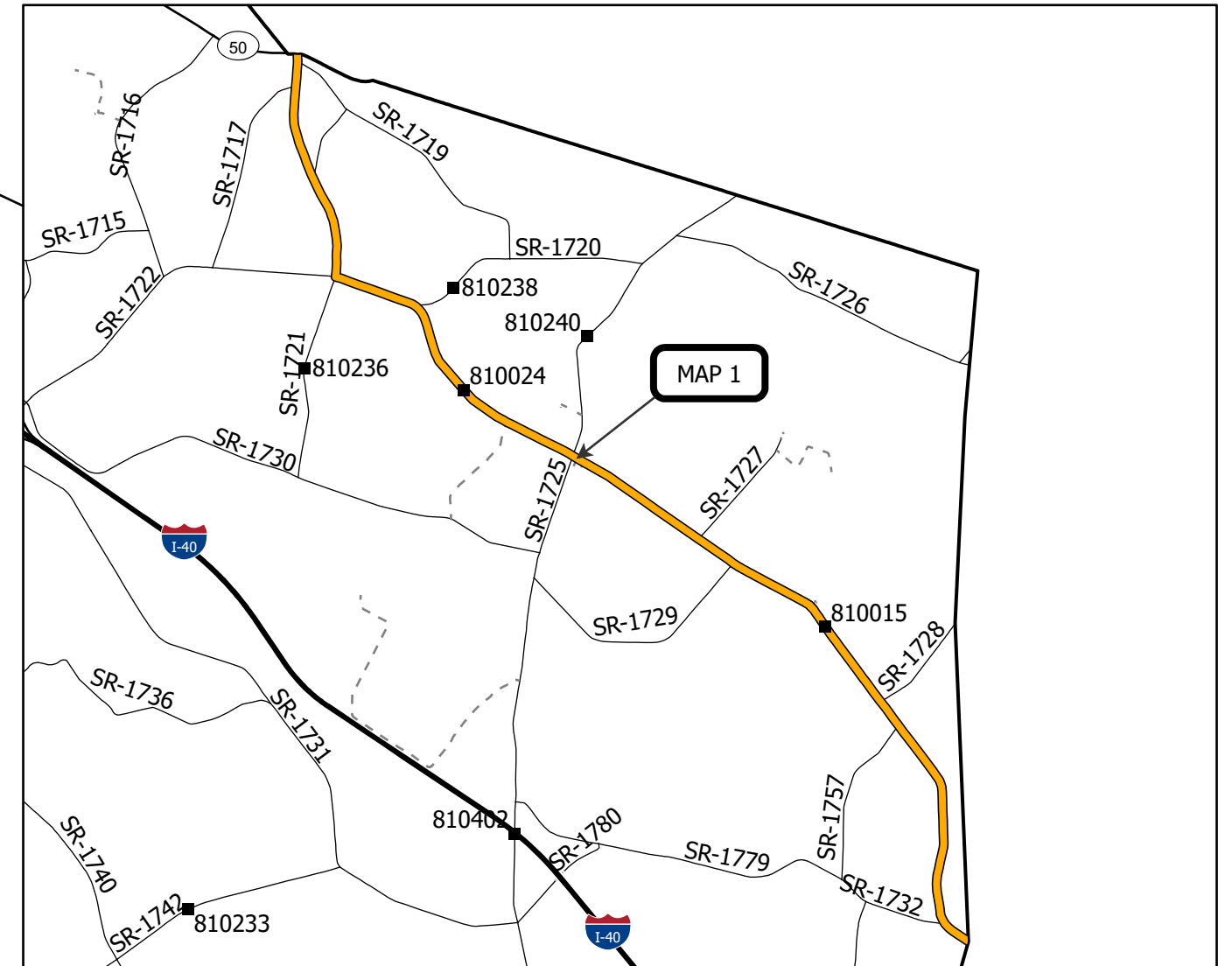
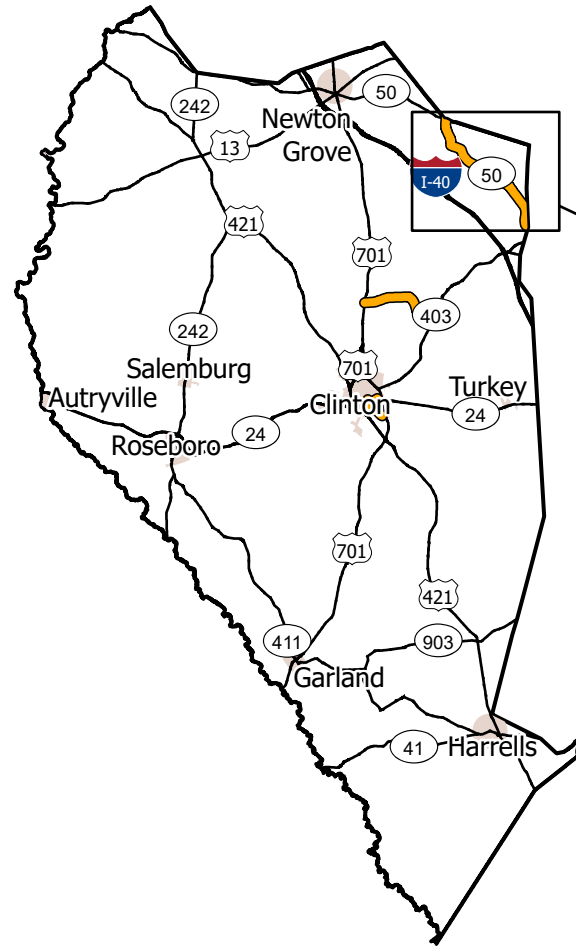
WBS NO: 2024CPT.03.06.10821, ETC.

CONTRACT: DC00446

STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS
SAMPSON COUNTY

TYPE OF WORK: RESURFACING

STATE	STATE PROJECT REFERENCE NUMBER	SHEET NO.
NC	2024CPT.03.06.10821, ETC.	1
STATE PROJECT NUMBER		DESCRIPTION



MAP	ROUTE	DESCRIPTION
1	NC-50 / SUTTONTOWN RD	FROM THE DUPLIN COUNTY LINE TO THE WAYNE COUNTY LINE [MP 0.00 - MP 8.39]

PROJECT LENGTH

MAP	MILES
1	8.39

Prepared in the Office of:
DIVISION OF HIGHWAYS
 5501 BARBADOS BLVD., CASTLE HAYNE, NC, 28429

This exhibit does not meet NC 47-30 requirements and therefore is not intended for design, construction, or recording or transfer of title.



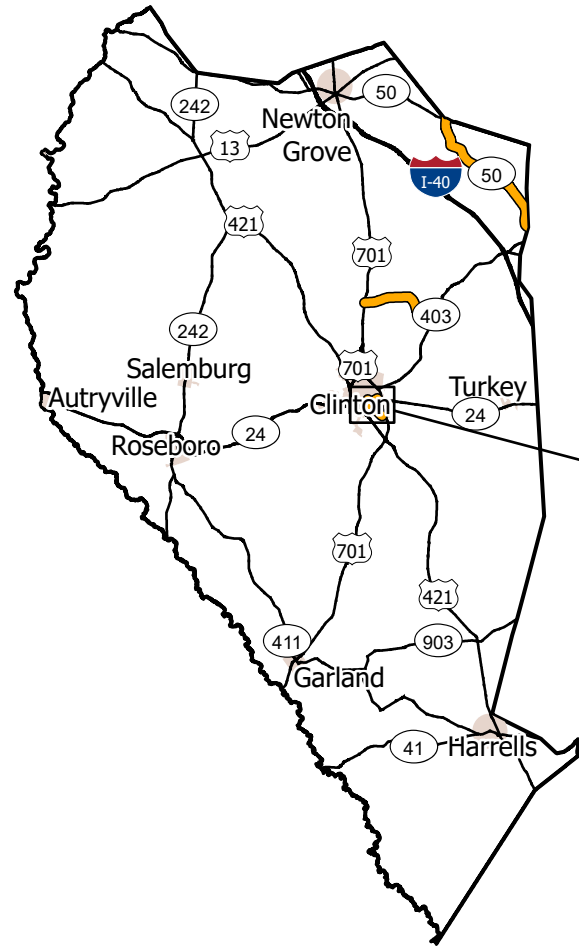
WBS NO: 2024CPT.03.06.10821, ETC.

CONTRACT: DC00446

STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS
SAMPSON COUNTY

TYPE OF WORK: RESURFACING

STATE	STATE PROJECT REFERENCE NUMBER	SHEET NO.
NC	2024CPT.03.06.10821, ETC.	3
STATE PROJECT NUMBER		DESCRIPTION



MAP	ROUTE	DESCRIPTION
3	SR-1231 / LISBON ST	FROM SR 1276 (SOUTHWEST BLVD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 1.24]
4	SR-1275 / E & W MORISEY BLVD	FROM SR 1855 (WARSAW RD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 0.95]

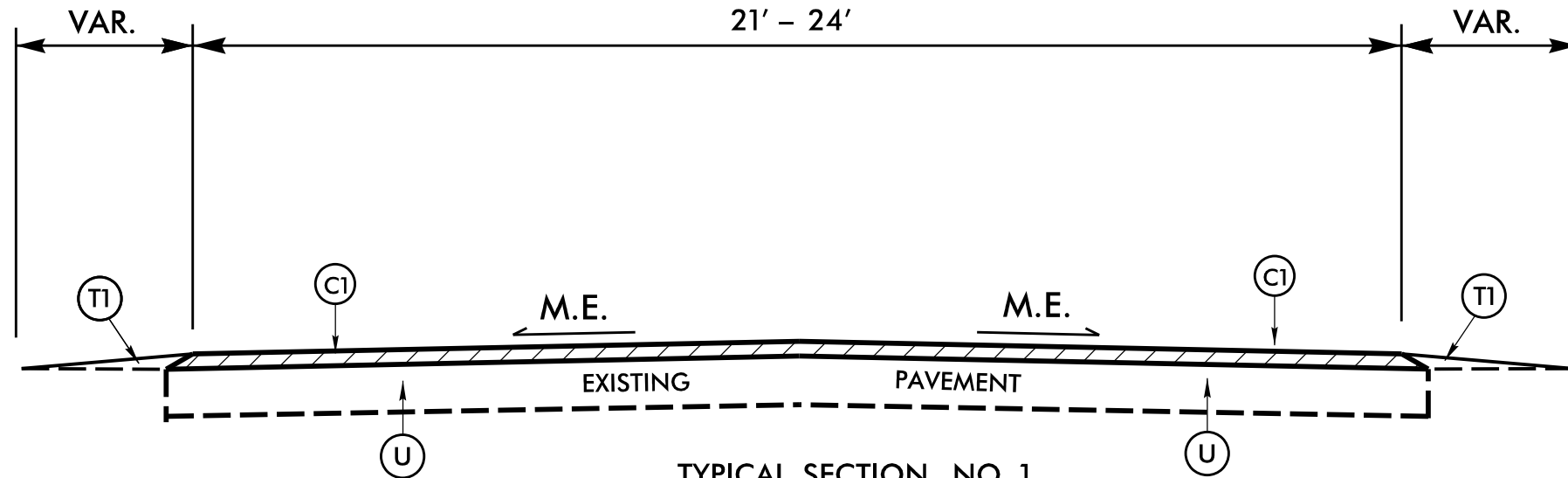
PROJECT LENGTH

MAP	MILES
3	1.24
4	0.95

Prepared in the Office of:
DIVISION OF HIGHWAYS
 5501 BARBADOS BLVD., CASTLE HAYNE, NC, 28429

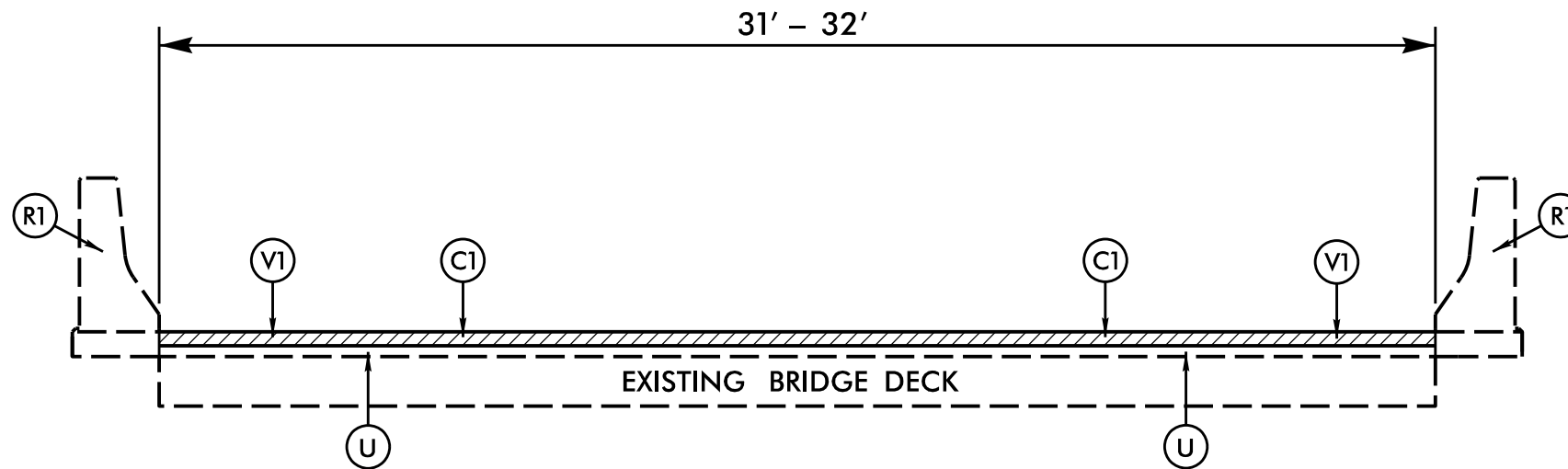
This exhibit does not meet NC 47-30 requirements and therefore is not intended for design, construction, or recording or transfer of title.





TYPICAL SECTION NO. 1

MAP NO. 1
 NC 50 (SUTTONTOWN RD.)
 MP 0.00 – MP 2.52
 MP 2.54 – MP 5.52
 MP 5.54 – MP 8.39



TYPICAL SECTION NO. 2

MAP NO. 1
 NC 50 (SUTTONTOWN RD.)
 SAMPSON BRIDGE NO. 15
 MP 2.52 – MP 2.54

MAP NO. 1
 NC 50 (SUTTONTOWN RD.)
 SAMPSON BRIDGE NO. 24
 MP 5.52 – MP 5.54

PAVEMENT SCHEDULE

C1	PROP. APPROX. 1 1/2" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ.YD.
C2	PROP. APPROX. 1 1/2" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ.YD.
C3	PROP. APPROX. 1" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ.YD.
D1	PROP. APPROX. 2 1/2" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 285 LBS. PER SQ.YD.
R1	EXISTING CONCRETE BRIDGE RAIL
R2	EXISTING CONCRETE 2'6" CURB AND GUTTER
V1	1.50" MILLING
V2	0" TO 1.50" MILLING
T1	SHOULDER RECONSTRUCTION (STATE FURNISHED BORROW)
U	EXISTING PAVEMENT

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

NOTES: SHOULDER WORK ON MAP NO. 1 – 4 AS NEEDED, TO BE DETERMINED BY ENGINEER.

2018 ROADWAY ENGLISH STANDARD DRAWINGS

EFF. 01-16-2018

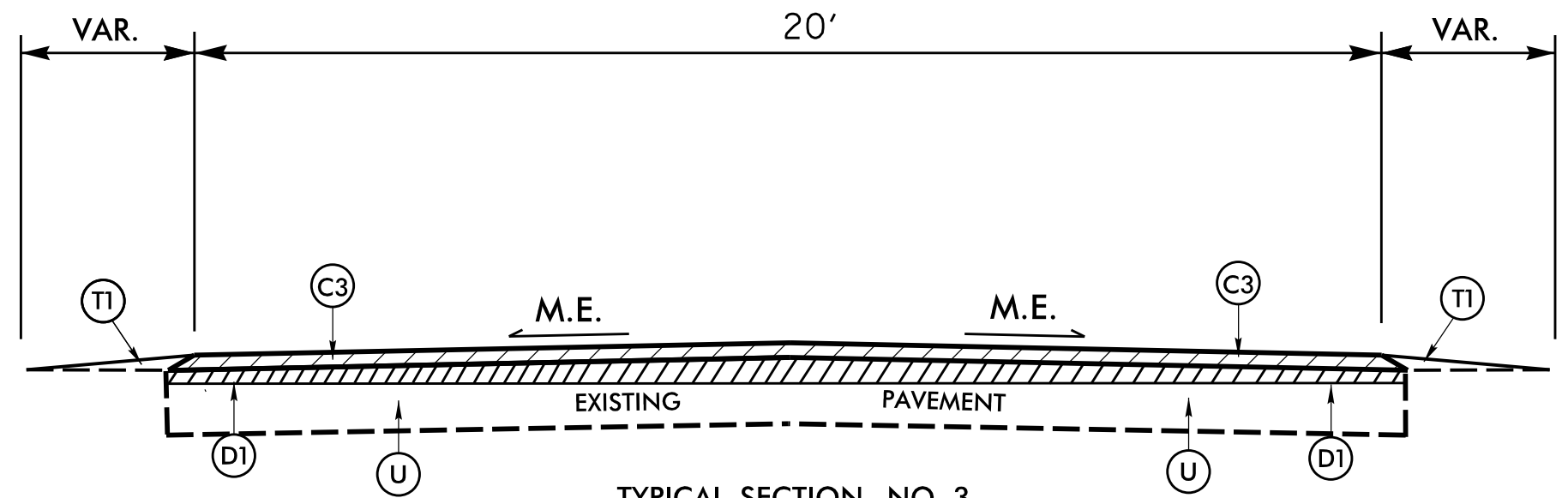
The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch – N. C. Department of Transportation – Raleigh, N. C., Dated January, 2018 are applicable to this project and by reference hereby are considered a part of these plans:

STD.NO.	TITLE
DIVISION 12 – PVMT MARKING, MARKERS AND DELINEATION	
1205.08	Pavement Markings – Symbols & Word Messages

REVISIONS

SECTION: \$\$\$\$\$\$
 DRAWING: \$\$\$\$\$\$
 TITLE: \$\$\$\$\$\$

PAVEMENT SCHEDULE	
C2	1 1/2" S9.5B
C3	1" S9.5B
D1	2 1/2" I19.0C
R2	EXISTING CONCRETE 2'6" CURB & GUTTER
V2	0" - 1.5" MILLING
T1	SHOULDER RECONST. (STATE FURNISHED BORROW)
U	EXISTING PAVEMENT

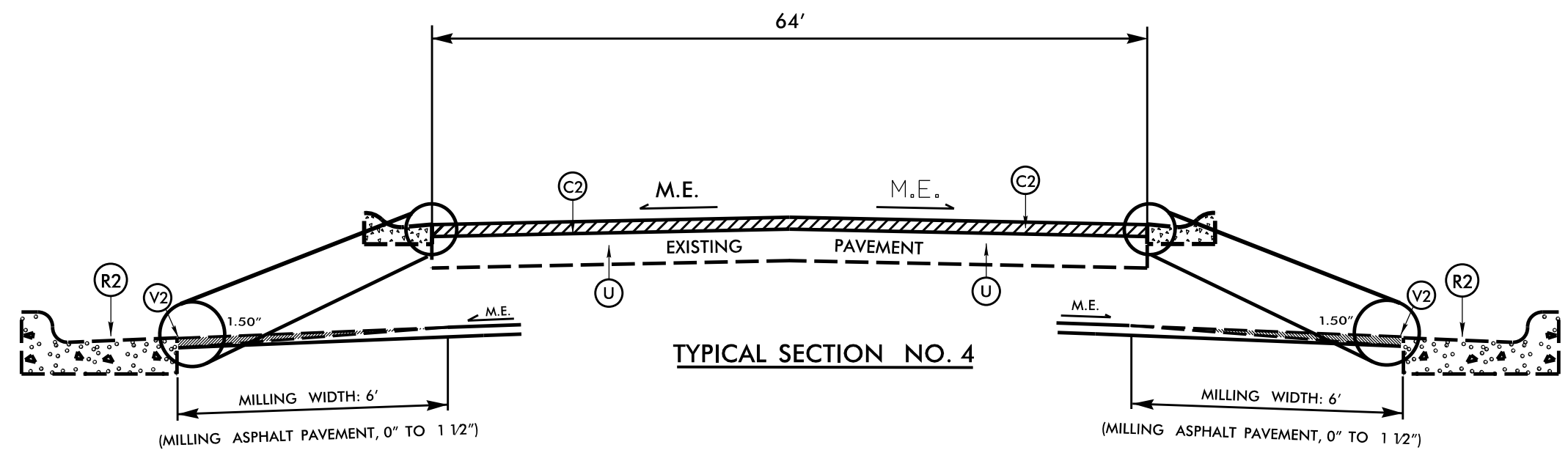


TYPICAL SECTION NO. 3

MAP NO. 2
SR 1748 (KING RD.)
MP 0.00 - MP 4.72

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

NOTES: SHOULDER WORK ON MAP NO. 1 - 4 AS NEEDED, TO BE DETERMINED BY ENGINEER.



TYPICAL SECTION NO. 4

MAP NO. 4
SR 1231 (LISBON ST.)
MP 0.00 - MP 1.24

MAP NO. 5
SR 1275 (E. & W. MORISEY BLVD.)
MP 0.00 - MP 0.95

REVISIONS

8/17/99
SECTION: 8000
DRAWN: JG
CHECKED: JG
DATE: 8/17/99

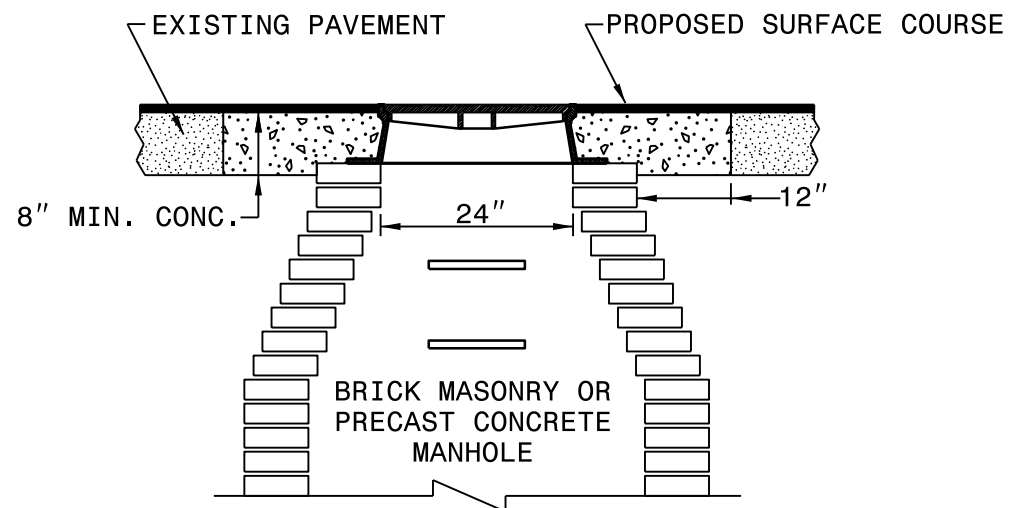
STATE OF
NORTH CAROLINA
DEPT. OF TRANSPORTATION
DIVISION OF HIGHWAYS
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

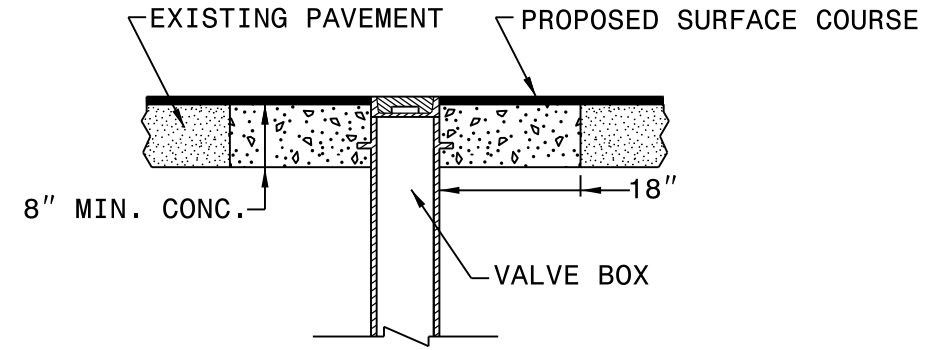
SHEET 1 OF 1
840D55

GENERAL NOTES:

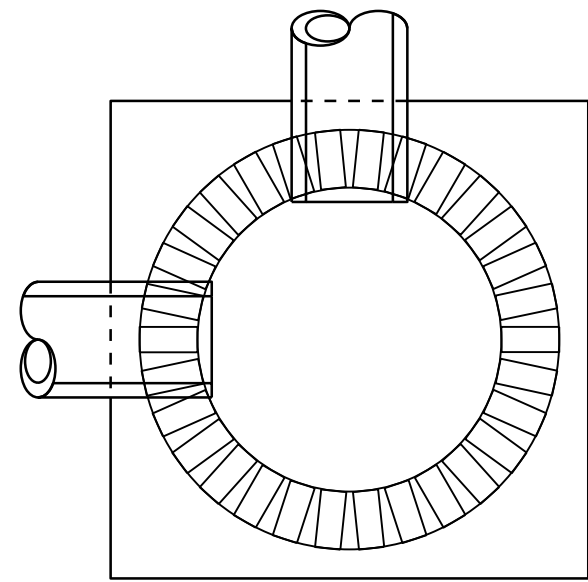
1. USE RAPID SET GROUT, MORTAR, OR CONCRETE THAT WILL TAKE FULL SET AND BECOME LOAD BEARING WITHIN SIXTY MINUTES OF PLACEMENT WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS $\frac{1}{2}$ " +/- $\frac{1}{8}$ "
7. CONSTRUCT AN ASPHALT RAMP IN ACCORDANCE WITH SECTION 858-3 OF THE 2018 STANDARD SPECIFICATIONS.



MANHOLE CONCRETE ENCASEMENT



VALVE BOX CONCRETE ENCASEMENT



ELEVATION VIEW

PLACE BRICK ACCORDING TO ELEVATION VIEW

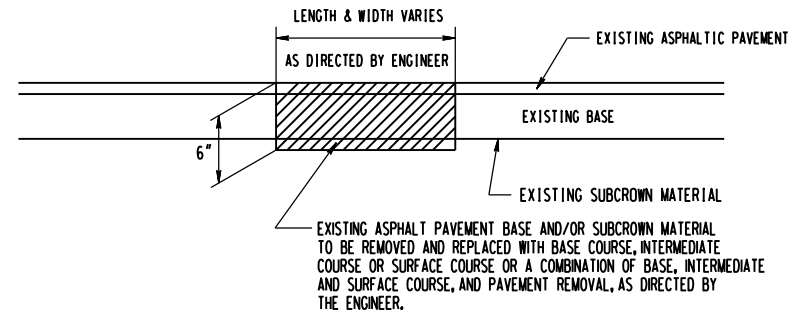
ENGLISH DETAIL DRAWING FOR
MANHOLE AND VALVE BOX ADJUSTMENTS

SHEET 1 OF 1
840D55

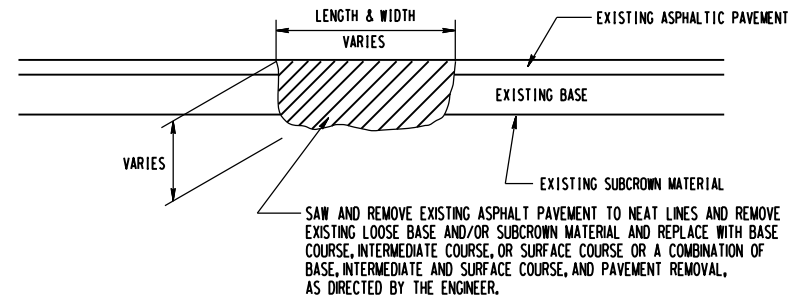
REVISIONS

28-NOV-2017 10:49
S:\Division Resurfacing\z Design Standards & CADD\CADD_Specs.Dwg\Tutorials_Curb, Etc.dwg
\$\$\$\$\$

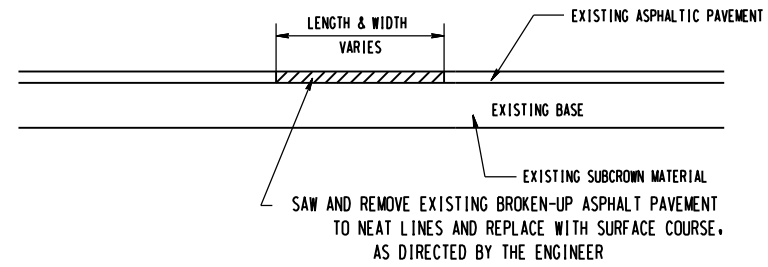
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



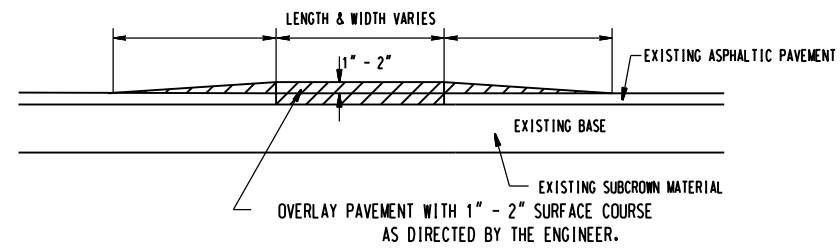
DETAIL NO. 1



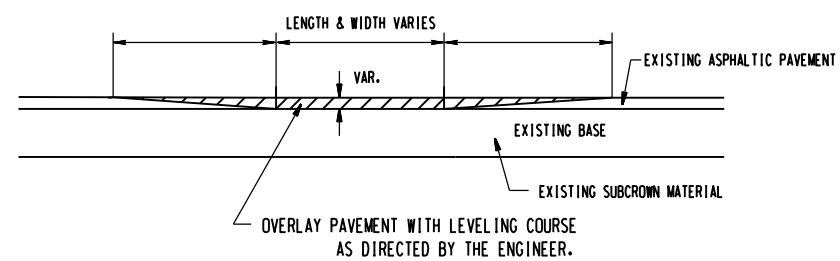
DETAIL NO. 2



DETAIL NO. 3



DETAIL NO. 4

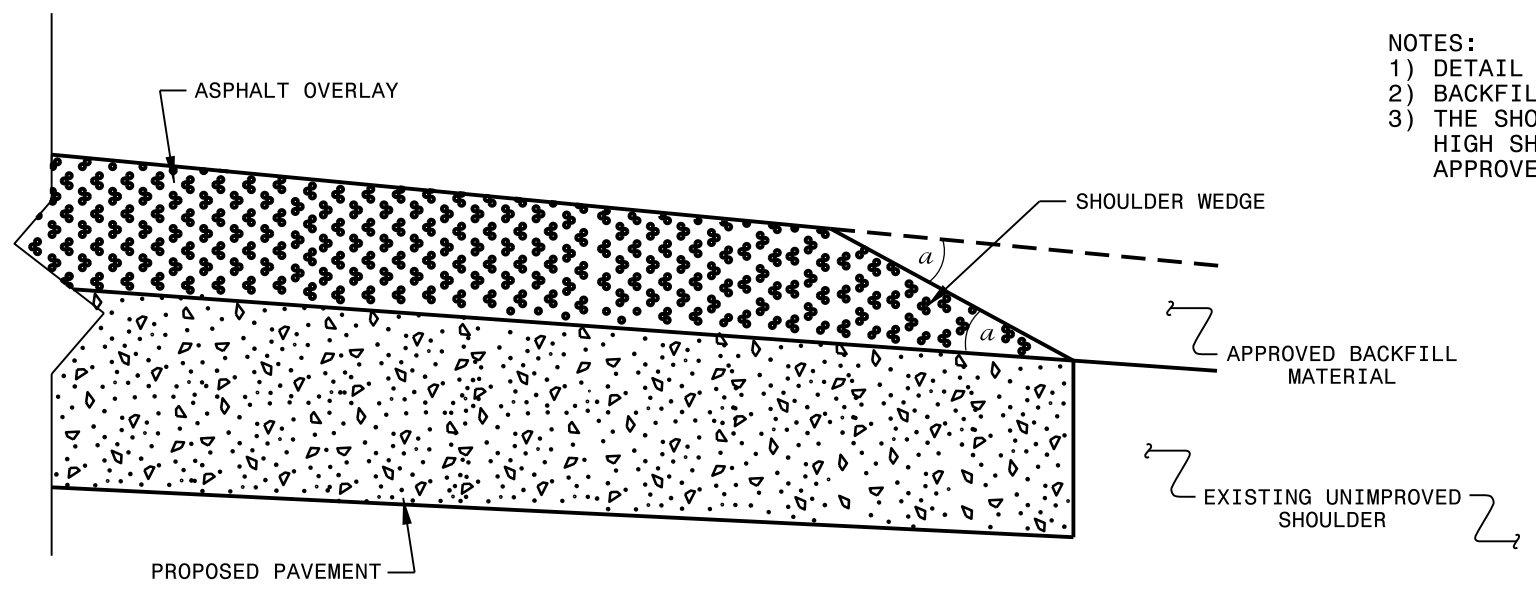


DETAIL NO. 5

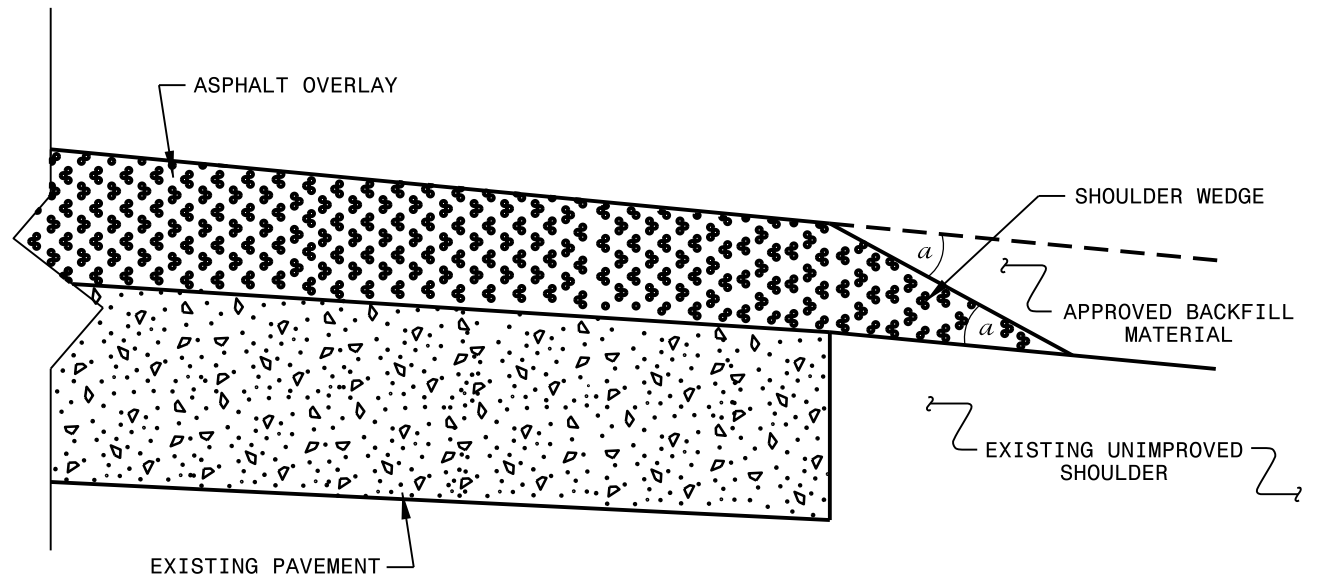
REVISIONS

20-APP-2015-1156
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 \$\$\$SUNRISE\$\$\$

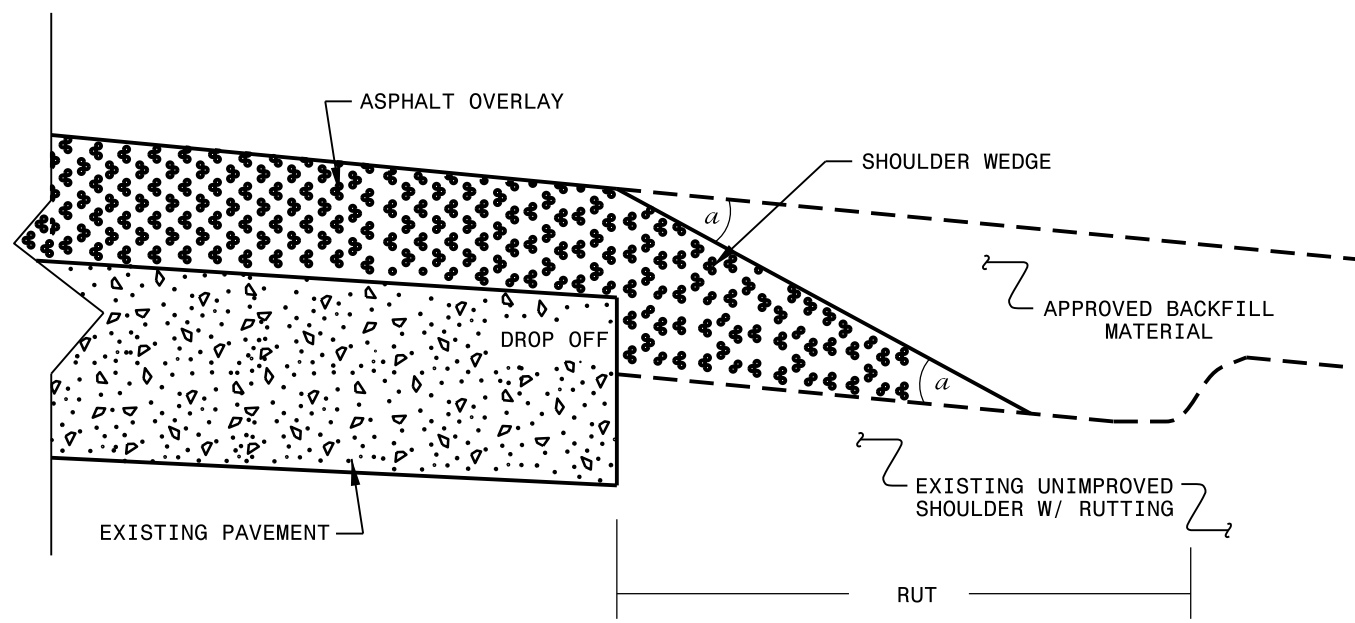
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT			
Office 919-707-6950		FAX 919-250-4119	
SHOULDER WEDGE DETAILS			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn			

27 JUN 2018 13:22
 C:\Users\jacob\Documents\Resurfacing Projects\Division 3\Sampson August 2018\Revised Shoulder Wedge Detail.dgn
 jacob

DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA

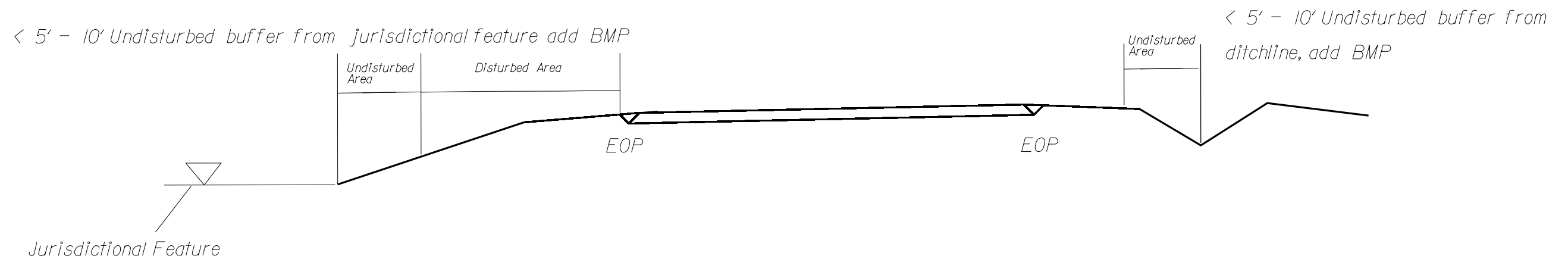
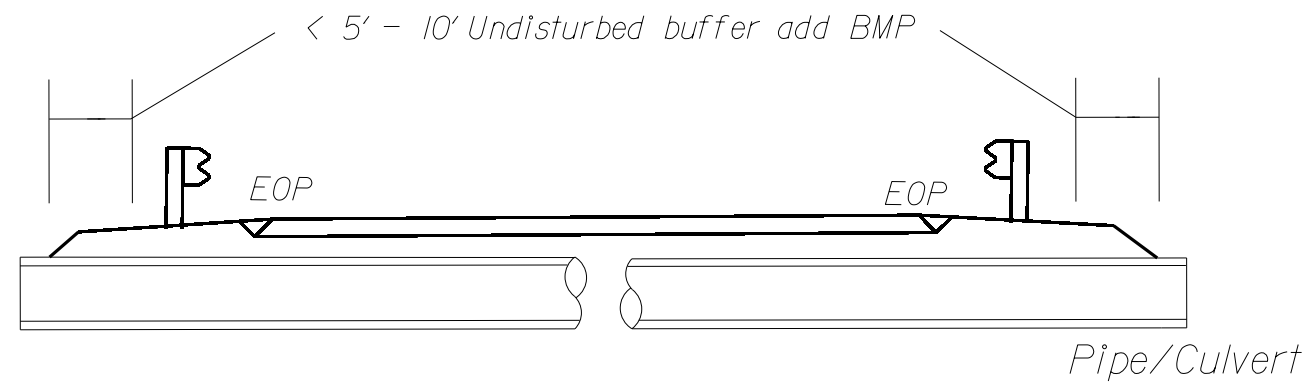
SOIL STABILIZATION TIMEFRAMES

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

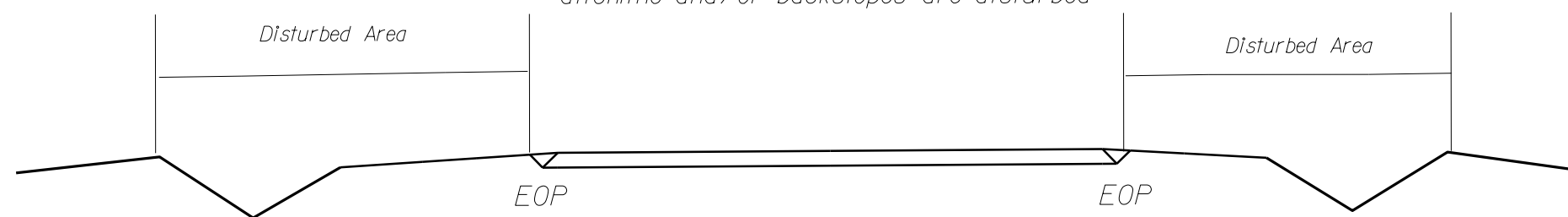
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

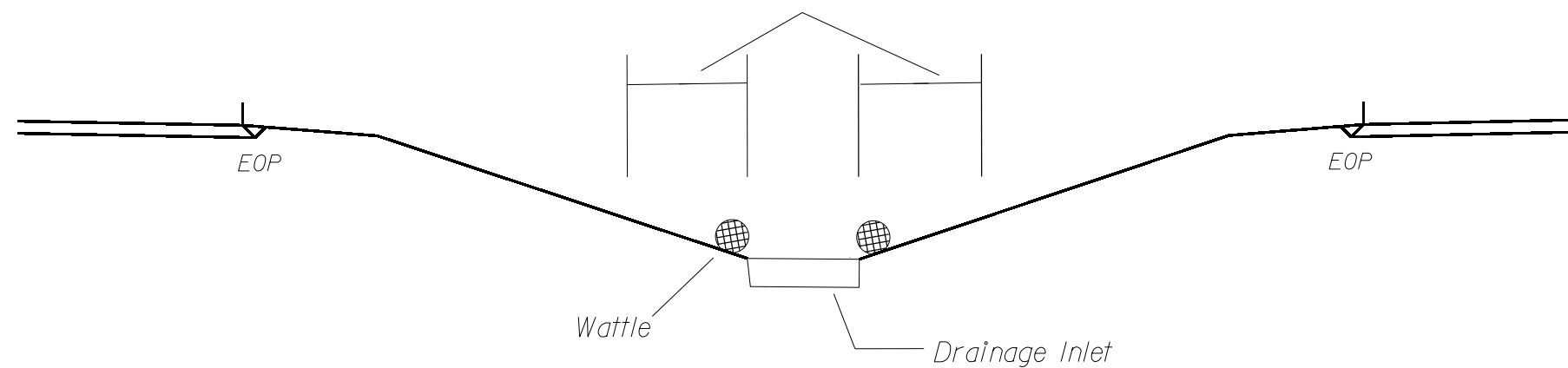
EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

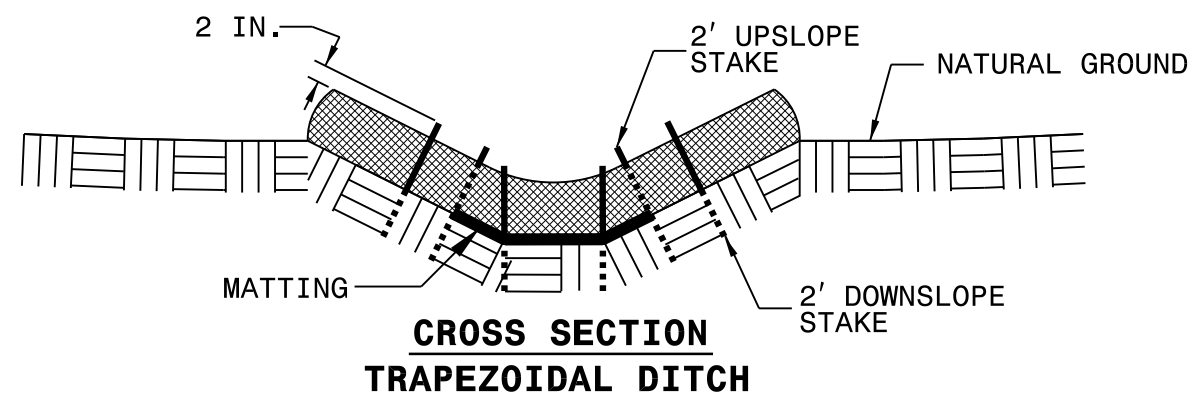
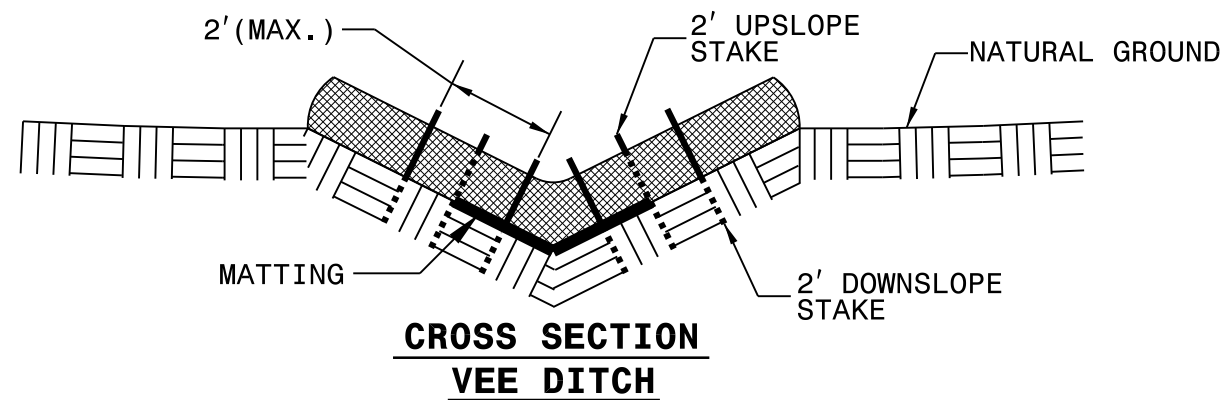
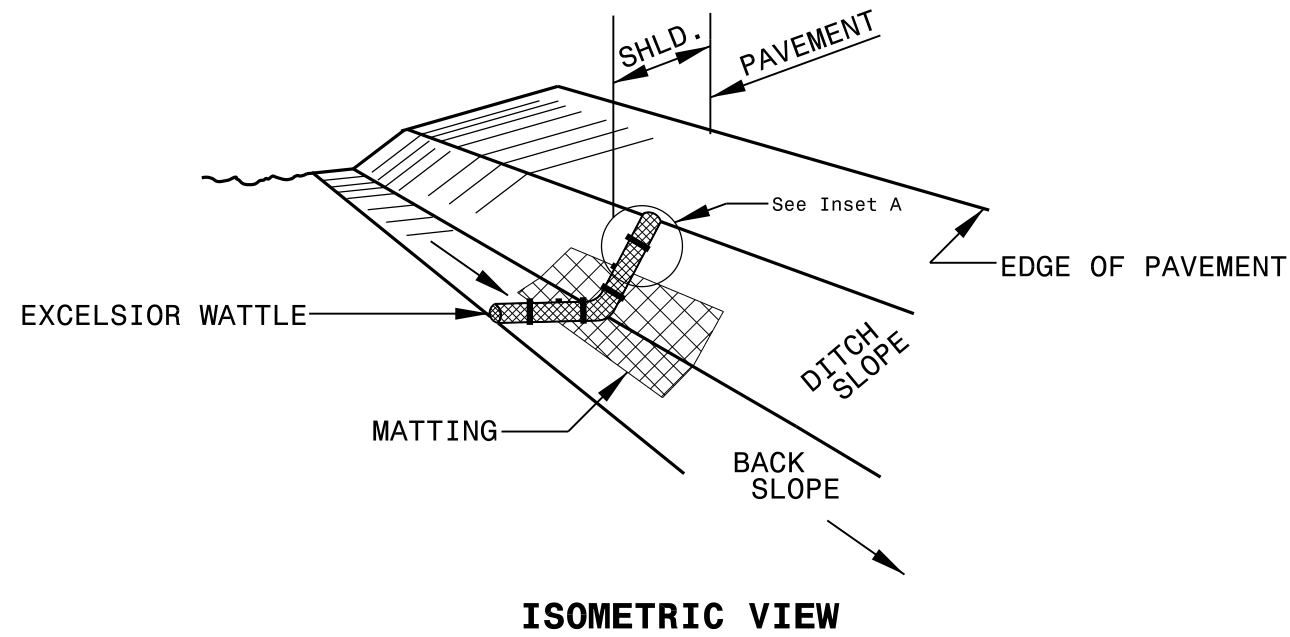


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

WATTLE DETAIL



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

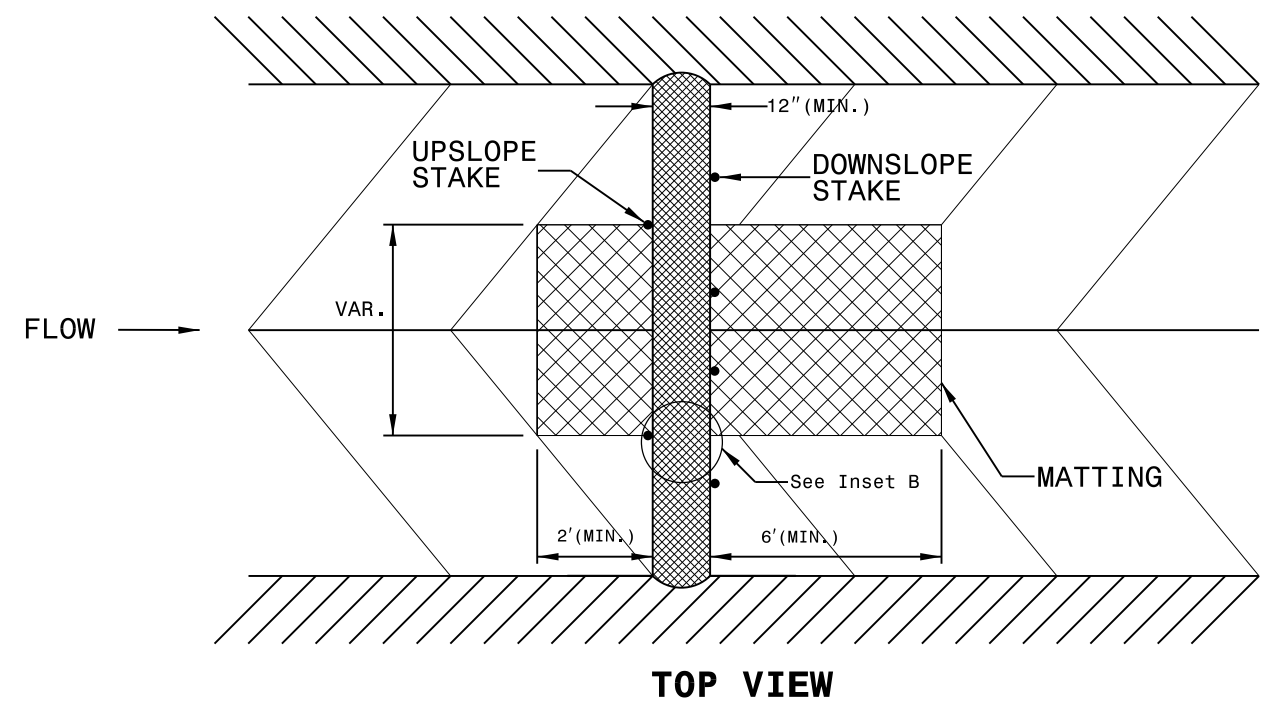
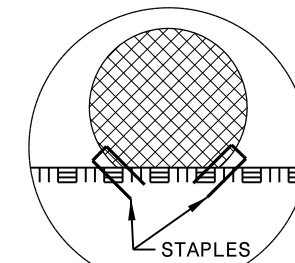
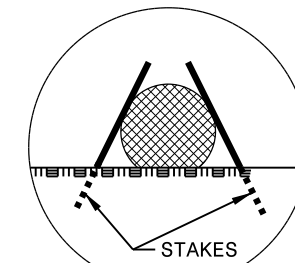
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



PROJECT NO.	SHEET NO.
2024CPT.03.06.10821, Etc.	9

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	BEGIN MP	END MP	0234000000-E	0264000000-E	1220000000-E	1297000000-E	1308000000-E	1330000000-E	1503000000-E	1519000000-E	1520000000-E	1523000000-E	1524000000-E	1575000000-E									
														STATE FURNISHED BORROW EXCAVATION	SHOULDER RECONSTRUCTION (STATE FURNISHED BORROW)	INCIDENTAL STONE BASE	1.5" MILLING	0" - 1.5" MILLING (GENERIC)	INCIDENTAL MILLING	INTERMEDIATE COURSE, I19.0C	SURFACE COURSE, S9.5B	LEVELING COURSE, S9.5B	SURFACE COURSE, S9.5C	LEVELING COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX									
														MI	FT																			
														CY	SMI	TON	SY	SY	SY	TON	TON	TON	TON	TON										
2024CPT.03.06.10821	Sampson	1	NC-50 / SUTTONTOWN RD	FROM THE DUPLIN COUNTY LINE TO THE WAYNE COUNTY LINE [MP 0.00 - MP 8.39]	1,2	2	2WU	NO	NO	8.39	21-32	0	8.39	1,270	16.78	132	739		934				9,943	10	594									
TOTAL FOR MAP NO. 1														8.39							1,270	16.78	132	739		934			9,943	10	594			
TOTAL FOR PROJ NO. 2024CPT.03.06.10821														8.39							1,270	16.78	132	739		934			9,943	10	594			
2024CPT.03.06.20821	Sampson	2	SR-1748 / KING RD	FROM NC HWY 403 (FAISON HIGHWAY) TO US HWY 701 (HOBSTON HIGHWAY) [MP 0.00 - MP 4.72]	3	2	2WU	NO	NO	4.72	20	0	4.72	3,935	9.44	122			888	8,207	3,148	5			612									
TOTAL FOR MAP NO. 2														4.72							3,935	9.44	122			888	8,207	3,148	5		612			
2024CPT.03.06.20821	Sampson	3	SR-1231 / LISBON ST	FROM SR 1276 (SOUTHWEST BLVD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 1.24]	4	2	2WU	NO	NO	1.24	36	0	1.24	50	0.76	20		5,375	1,292			2,276	5		151									
TOTAL FOR MAP NO. 3														1.24							50	0.76	20		5,375	1,292		2,276	5		151			
2024CPT.03.06.20821	Sampson	4	SR-1275 / E&W MORISEY BLVD	FROM SR 1855 (WARSAW RD) TO SR 1214 (E. ELIZABETH ST) [MP 0.00 - MP 0.95]	4	2	2WU	NO	NO	0.95	36	0	0.95	30	0.43			4,709	745			1,744	5		119									
TOTAL FOR MAP NO. 4														0.95							30	0.43			4,709	745		1,744	5		119			
TOTAL FOR PROJ NO. 2024CPT.03.06.20821														6.91							4,015	10.63	142		10,084	2,925	8,207	7,168	15		882			
GRAND TOTAL														15.30							5,285	27.41	274		739		10,084	3,859	8,207	7,168	15	9,943	10	1,476

PROJECT NO.	SHEET NO.
2024CPT.03.06.10821, Etc.	10

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	BEGIN MP	END MP	1705500000-E	2845000000-N	2850000000-N	6000000000-E	6009000000-E	6012000000-E	6015000000-E	6018000000-E	6021000000-E	6036000000-E	6042000000-E	6071010000-E				
														PATCHING EXISTING PAVEMENT (MILL)	ADJUSTMENT OF METER OR VALVE BOX	REMOVE AND REPLACE EXISTING MANHOLES	TEMPORARY SILT FENCE	STONE FOR EC CLASS B	SEDIMENT CONTROL STONE	TEMPORARY MULCHING	SEED FOR TEMPORARY SEEDING	FERTILIZER FOR TEMPORARY SEEDING	MATTING FOR EROSION CONTROL	1/4" HARDWARE CLOTH	WATTLE				
														MI	FT			TON	EA	EA	LF	TON	TON	ACR	LBS	TON	SY	LF	LF
2024CPT.03.06.10821	Sampson	1	NC-50 / SUTTONTOWN RD	FROM THE DUPLIN COUNTY LINE TO THE WAYNE COUNTY LINE [MP 0.00 - MP 8.39]	1,2	2	2WU	NO	NO	8.39	21-32	0	8.39	138			830	210	210	8.30	415	2.00	60	415	130				
TOTAL FOR MAP NO. 1										8.39				138			830	210	210	8.30	415	2.00	60	415	130				
TOTAL FOR PROJ NO. 2024CPT.03.06.10821										8.39				138			830	210	210	8.30	415	2.00	60	415	130				
2024CPT.03.06.20821	Sampson	2	SR-1748 / KING RD	FROM NC HWY 403 (FAISON HIGHWAY) TO US HWY 701 (HOBBDON HIGHWAY) [MP 0.00 - MP 4.72]	3	2	2WU	NO	NO	4.72	20	0	4.72	270			466	115	115	4.70	235	1.20	30	235	70				
TOTAL FOR MAP NO. 2										4.72				270			466	115	115	4.70	235	1.20	30	235	70				
2024CPT.03.06.20821	Sampson	3	SR-1231 / LISBON ST	FROM SR 1276 (SOUTHWEST BLVD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 1.24]	4	2	2WU	NO	NO	1.24	36	0	1.24	49	13	19	40	10	10	0.40	20	0.10	10	20	10				
TOTAL FOR MAP NO. 3										1.24				49	13	19	40	10	10	0.40	20	0.10	10	20	10				
2024CPT.03.06.20821	Sampson	4	SR-1275 / E&W MORISEY BLVD	FROM SR 1855 (WARSAW RD) TO SR 1214 (E. ELIZABETH ST) [MP 0.00 - MP 0.95]	4	2	2WU	NO	NO	0.95	36	0	0.95	120	7	17	20	5	5	0.20	10	0.05	10	10	10				
TOTAL FOR MAP NO. 4										0.95				120	7	17	20	5	5	0.20	10	0.05	10	10	10				
TOTAL FOR PROJ NO. 2024CPT.03.06.20821										6.91				439	20	36	526	130	130	5.30	265	1.35	50	265	90				
GRAND TOTAL										15.30				577	20	36	1,356	340	340	13.60	680	3.35	110	680	220				

PROJECT NO.	SHEET NO.
2024CPT.03.06.10821, Etc.	11

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	BEGIN MP	END MP	6084000000-E	6090000000-E	6093000000-E	6117000000-N	7444000000-E							
														SEED & MULCHING	SEED FOR REPAIR SEEDING	FERTILIZER FOR REPAIR SEEDING	RESPONSE FOR EROSION CONTROL	INDUCTIVE LOOP SAWCUT							
														MI	FT										
														AC	LB	TON	EA	LF							
2024CPT.03.06.10821	Sampson	1	NC-50 / SUTTONTOWN RD	FROM THE DUPLIN COUNTY LINE TO THE WAYNE COUNTY LINE [MP 0.00 - MP 8.39]	1,2	2	2WU	NO	NO	8.39	21-32	0	8.39	8.00	415	2.00	17								
TOTAL FOR MAP NO. 1														8.39											
TOTAL FOR PROJ NO. 2024CPT.03.06.10821														8.39											
2024CPT.03.06.20821	Sampson	2	SR-1748 / KING RD	FROM NC HWY 403 (FAISON HIGHWAY) TO US HWY 701 (HOBBDON HIGHWAY) [MP 0.00 - MP 4.72]	3	2	2WU	NO	NO	4.72	20	0	4.72	4.50	235	1.20	9								
TOTAL FOR MAP NO. 2														4.72											
2024CPT.03.06.20821	Sampson	3	SR-1231 / LISBON ST	FROM SR 1276 (SOUTHWEST BLVD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 1.24]	4	2	2WU	NO	NO	1.24	36	0	1.24	0.37	20	0.10	2	200							
TOTAL FOR MAP NO. 3														1.24											
2024CPT.03.06.20821	Sampson	4	SR-1275 / E&W MORISEY BLVD	FROM SR 1855 (WARSAW RD) TO SR 1214 (E. ELIZABETH ST) [MP 0.00 - MP 0.95]	4	2	2WU	NO	NO	0.95	36	0	0.95	0.20	10	0.05	2	200							
TOTAL FOR MAP NO. 4														0.95											
TOTAL FOR PROJ NO. 2024CPT.03.06.20821														6.91											
GRAND TOTAL														15.30							13.07	680	3.35	30	400

PROJECT NO.	SHEET NO.
2024CPT.03.06.10821, Etc.	12

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	4413000000-E	4457000000-N	4510000000-N	4685000000-E		4695000000-E	
												WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	8" X 90 M WHITE THERMO	8" X 90 M YELLOW THERMO
												SF	LS	HR	LF	LF	LF	LF
2024CPT.03.06.10821	Sampson	1	NC-50 / SUTTONTOWN RD	FROM THE DUPLIN COUNTY LINE TO THE WAYNE COUNTY LINE [MP 0.00 - MP 8.39]	1,2	2	2WU	8.39	21-32	0	8.39	928	0.49		87,518	52,428		
TOTAL FOR MAP NO. 1												928	0.49		87,518	52,428		
TOTAL FOR PROJ NO. 2024CPT.03.06.10821												928	0.49		87,518	52,428		
												139,946						
2024CPT.03.06.20821	Sampson	2	SR-1748 / KING RD	FROM NC HWY 403 (FAISON HIGHWAY) TO US HWY 701 (HOBBTON HIGHWAY) [MP 0.00 - MP 4.72]	3	2	2WU	4.72	20	0	4.72	528	0.32		49,204	26,908		
TOTAL FOR MAP NO. 2												528	0.32		49,204	26,908		
2024CPT.03.06.20821	Sampson	3	SR-1231 / LISBON ST	FROM SR 1276 (SOUTHWEST BLVD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 1.24]	4	2	2WU	1.24	36	0	1.24		0.11	80	11,425	8,698	173	67
TOTAL FOR MAP NO. 3													0.11	80	11,425	8,698	173	67
2024CPT.03.06.20821	Sampson	4	SR-1275 / E&W MORISEY BLVD	FROM SR 1855 (WARSAW RD) TO SR 1214 (E. ELIZABETH ST) [MP 0.00 - MP 0.95]	4	2	2WU	0.95	36	0	0.95		0.08	40	7,262	9,460	1,600	
TOTAL FOR MAP NO. 4													0.08	40	7,262	9,460	1,600	
TOTAL FOR PROJ NO. 2024CPT.03.06.20821												528	0.51	120	67,891	45,066	1,773	67
												112,957						1,840
GRAND TOTAL												1,456	1	120	155,409	97,494	1,773	67
												252,903						1,840

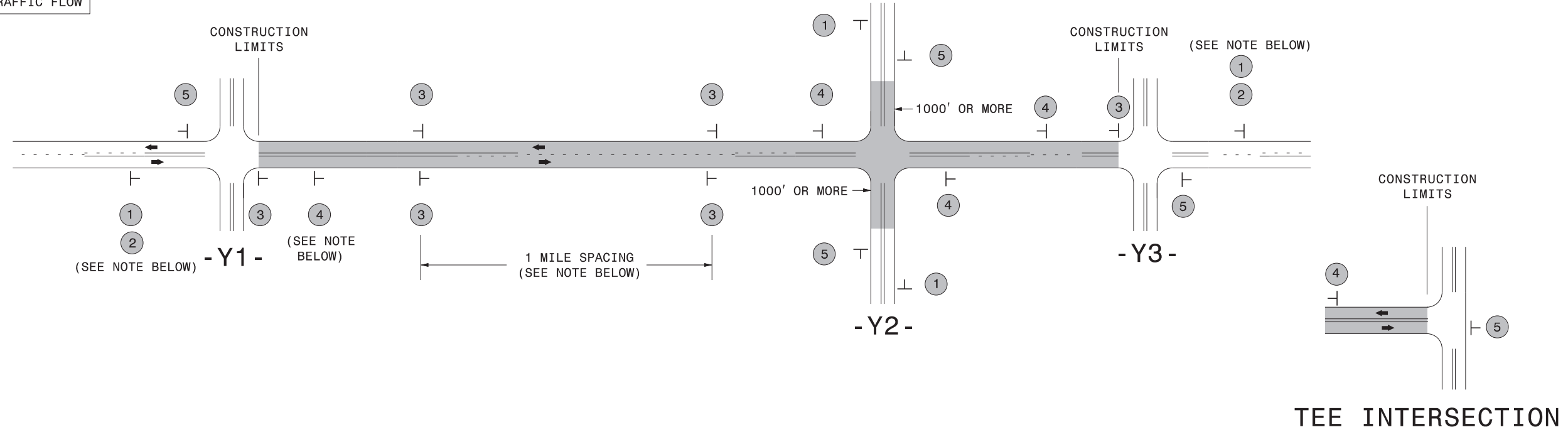
PROJECT NO.	SHEET NO.
2024CPT.03.06.10821, Etc.	13

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	4704000000-E	4709000000-E	4720000000-E	4725000000-E				4900000000-N		4905100000-N			
												THERMOPLAS TIC PAVEMENT MARKING LINES (16" X 90 MILS)	THERMOPLAS TIC PAVEMENT MARKING LINES (24" X 90 MILS)	THERMO RXR 90 MILS	THERMO LT ARROW (90 MIL)	THERMO STR & RT ARROW (90 MIL)	THERMO STR & LT ARROW (90 MIL)	THERMO RT ARROW (90 MIL)	YELLOW & YELLOW MARKERS	CRYSTAL & RED MARKERS	NON CAST IRON SNOWPLOWABLE MARKERS (Y/Y)			
												MI	FT											
2024CPT.03.06.10821	Sampson	1	NC-50 / SUTTONTOWN RD	FROM THE DUPLIN COUNTY LINE TO THE WAYNE COUNTY LINE [MP 0.00 - MP 8.39]	1,2	2	2WU	8.39	21-32	0	8.39											550		
TOTAL FOR MAP NO. 1								8.39														550		
TOTAL FOR PROJ NO. 2024CPT.03.06.10821								8.39															550	
2024CPT.03.06.20821	Sampson	2	SR-1748 / KING RD	FROM NC HWY 403 (FAISON HIGHWAY) TO US HWY 701 (HOBBDON HIGHWAY) [MP 0.00 - MP 4.72]	3	2	2WU	4.72	20	0	4.72											310		
TOTAL FOR MAP NO. 2								4.72															310	
2024CPT.03.06.20821	Sampson	3	SR-1231 / LISBON ST	FROM SR 1276 (SOUTHWEST BLVD) TO SR 1214 (E. ELIZABETH ST.) [MP 0.00 - MP 1.24]	4	2	2WU	1.24	36	0	1.24	80	131	4	8	4	4					75	12	
TOTAL FOR MAP NO. 3								1.24				80	131	4	8	4	4						75	12
2024CPT.03.06.20821	Sampson	4	SR-1275 / E&W MORISEY BLVD	FROM SR 1855 (WARSAW RD) TO SR 1214 (E. ELIZABETH ST) [MP 0.00 - MP 0.95]	4	2	2WU	0.95	36	0	0.95	80	173	4	5	2	2					65	12	
TOTAL FOR MAP NO. 4								0.95				80	173	4	5	2	2	1					65	12
TOTAL FOR PROJ NO. 2024CPT.03.06.20821								6.91				160	304	8	13	6	6	1					450	24
												26				474								
GRAND TOTAL								15.30				160	304	8	13	6	6	1				450	24	550
												26				474								

SIGNING FOR RESURFACING PROJECTS

LEGEND
 ┆ STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

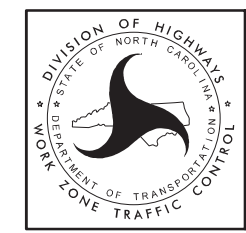
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING